



CITY OF BELLEVILLE
Rod Bovay RPP
Chief Administrative Officer
Report No. CAO 2021- 02

February 8, 2021

To: Mayor and Members of Council

Subject: Renovations to City Hall Council Chambers

Recommendation:

“THAT in accordance with Section 30.3 Sole and Single Sourcing – Approval and Reporting of the City’s Purchasing By-law Number 2020-09, the quotation from AVIAS for the installation of digital infrastructure and related electronic equipment within City Hall, in the amount of \$75,000.00 plus \$9,750.00 HST, for a total of \$84,750.00 be accepted, and that the Mayor and City Clerk be authorized to sign the Acceptance Agreement on behalf of the Corporation of the City of Belleville and the City Clerk be authorized to affix the Corporate Seal.”

AND that the 2020 Capital Budget Item No. 1.039 be amended by adding \$130,000 from the Capital Levy Reserve, to the project Budget.”

Strategic Alignment:

The City of Belleville’s Strategic Plan identifies nine strategic themes. The recommendation within this report aligns with the City’s strategic themes of Infrastructure, City Centre Revitalization and Community Health, Safety and Security.

Background:

City Council approved the request for tender submission from Tom Belch & Sons Building Contractors for the Renovations to City Hall Council Chambers in the amount of \$235,900.00 plus \$30,667.00 HST, for a total of \$266,567.00. Subsequent to this award the City has been advised that our application to the Enabling Accessibility Fund, administered by Employment and Social Development Canada, for \$92,500.00 has been denied.

As part of the redesign of the Council Chambers, provision was made to physically accommodate upgrades to the digital infrastructure, although no specific upgrades were included in the contract. The City's IT Section has been reviewing the existing digital capacity within the Chamber and City Hall in general. In order to support existing and future digital requirements it is recommended that upgrades be made at this time within the Chamber and in other strategic locations within City Hall. This would include upgrades to fibre optics to the Council Chamber from the first floor, clean up of old cabling and obsolete electronics within the Chamber and provision of new video screens, presentation equipment and components for electronic voting at Council meetings.

Given the specialized nature of the work and components staff have been working with AVIAS who previously installed the existing hardware & microphones in the Council Chambers, which are proposed to be reused. The appropriate Sole Source justification is attached to this report.

In addition, as part of the Council Chamber redesign, the architect suggested that new furniture be procured to complete the redesign. The recommended furniture was not part of the redesign tender and is proposed to be acquired through a Request for Quotation process. The estimated cost of the new furniture is approximately \$48,000.00.

Analysis:

Staff of the City's Building Department is supportive of undertaking the upgrades to fibre infrastructure while the reconstruction of the Council Chamber is underway as this will lessen the need for disturbing the completed work in future years.

They have also indicated that many of the gallery chairs, which have been in use since the City Hall reconstruction was completed in 1988, are at their end of their service life, as are many of the same style of chairs used in the SMB room on the third floor. It is proposed that chairs from the Council Chamber that are still in good shape be moved to the SMB to supplement the remaining good quality chairs in that meeting room. This should provide several years of service life of the remaining chairs in the SMB room.

Conclusion:

City staff recommends that Council approve the recommended upgrades to the digital infrastructure within City Hall, as well as the purchase of new furniture for the renovated Council Chambers.

Respectfully submitted,



Rod Bovay RPP
Chief Administrative Officer

CITY OF BELLEVILLE
SINGLE/SOLE SOURCING CHECKLIST

This checklist is a guideline to assist in determining if a purchase of goods and/or services qualifies as a single or sole source purchase. If further clarification is required please contact Purchasing Services.

Single and Sole Source purchases must be made in accordance with section 30.1 and 30.2 of the Procurement Policy.

Purchasing Services shall assist the Department in making the determination that a purchase meets the criteria of a sole or single source purchase by the following process:

1. Single Sourcing: (check appropriate item below)

Single source means that there is more than one source of supply in the open market, but only one source is recommended due to predetermined and approved specifications.

The procurement may be conducted using a single Source process if the goods and/or services are available from more than one source but there are valid and sufficient reasons for selecting one supplier in particular, as follows:

- An attempt to acquire the required goods and/or services by soliciting competitive bids has been made in good faith, but has failed to identify any, willing and compliant, competitive suppliers;
- For additional orders/deliveries by the original supplier of goods or services that were not included in the initial procurement if a change of supplier for such additional goods or services: (a) cannot be made for economic or technical reasons such as requirements of interchangeability or interoperability with existing equipment, software, services or installations procured under the initial procurement; and (b) would cause significant inconvenience or substantial duplication of costs for the procuring entity;
- for purchases made under exceptionally advantageous conditions that only arise in the very short term in the case of unusual disposals such as those arising from liquidation, auction, receivership or bankruptcy, but not for routine purchases from regular suppliers;
- It is advantageous to the City to acquire the goods and/or services from a supplier pursuant to the procurement process conducted by another public body;
- It is advantageous to the City to acquire the goods and/or services directly from another public body or public service body;
- Another organization is funding or substantially funding the acquisition and has determined the supplier, and the terms and conditions of the commitment into which the City will enter are acceptable to the City;
- Where due to abnormal market conditions, the goods and/or services required are in short supply.
- Section 21. from the Purchasing by-law where the goods and/or services are considered an extension of a previous Purchase using a competitive Bid process and the previous supplier would be considered appropriate as a single source procurement due to the particular nature of the extension.

CITY OF BELLEVILLE
SINGLE/SOLE SOURCING CHECKLIST

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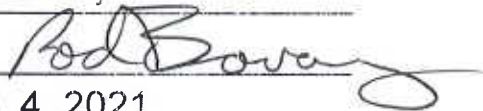
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- For additional orders/deliveries by the original supplier of goods or services that were not included in the initial procurement if a change of supplier for such additional goods or services: (a) cannot be made for economic or technical reasons such as requirements of interchangeability or interoperability with existing equipment, software, services or installations procured under the initial procurement; and (b) would cause significant inconvenience or substantial duplication of costs for the procuring entity;
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- Another organization is funding or substantially funding the acquisition and has determined the supplier, and the terms and conditions of the commitment into which the City will enter are acceptable to the City;
- Where due to abnormal market conditions, the goods and/or services required are in short supply.
- Section 21. from the Purchasing by-law where the goods and/or services are considered an extension of a previous Purchase using a competitive Bid process and the previous supplier would be considered appropriate as a single source procurement due to the particular nature of the extension.

SUMMARY OF RATIONALE:

Staff are proposing that the electronic upgrades in the Council Chamber be completed by AVIAS due the fact this company installed existing hardware components and we are attempting to reuse as many components as possible. Their familiarity with current setup will reduce costs.

Name: Rod Bovay

Signature: 

Date: Feb. 4, 2021

2. Sole Sourcing: (check appropriate item below)

The procurement may be conducted using a Sole Source process if the goods and/or services are available from only one supplier by reason of:

- Statutory or market based monopoly;
- Competition is precluded due to the application of any Act or legislation or because of the existence of patent rights, copyrights, license, technical secrets or controls of raw material; or
- The complete item, service, or system is unique to one supplier and no alternative or substitute exists.

SUMMARY OF RATIONALE

Charge Account Number: _____

Name: _____

Signature: _____

Date: _____



APPROVAL BLOCK	
CAO	<i>PB</i>
DCS	<i>[Signature]</i>
DEDS	<i>[Signature]</i>
GMT&OS	<i>[Signature]</i>

CITY OF BELLEVILLE
Christine Stewart, Deputy City Clerk
On Behalf of the Transportation Committee
Report No. DCC-2021-01
February 8, 2021

To: Mayor and Members of Council

Subject: City of Belleville - Transportation Committee

Recommendation:

“THAT Council approve the recommendations of the Transportation Committee for those items outlined in the Deputy City Clerk Report No.DCC-2021-01 City of Belleville Transportation Committee.

Strategic Plan Alignment:

The City of Belleville's Strategic Plan identifies nine strategic themes. The recommendation within this report aligns with the City's strategic theme "Transportation and Mobility" and the City's strategic objective to "Plan and develop a safe and efficient road and transportation system that addresses the needs of our residents and businesses".

Background:

The Transportation Committee met on January 27, 2021 and reviewed traffic issues as outlined in the meeting agenda.

Analysis:

The Transportation Committee met to review the items on the agenda and as a result, the following recommendations are presented to Council for **approval:**

(A) Proposed Amendment to Traffic By-law 12967 - Schedule Z

THAT pursuant to Report TC-GMTOS-2021-01 (Mitchell Road and Elmwood Drive – Heavy Truck Traffic Restrictions) THAT the Transportation Committee recommends to Council that Traffic By-law 12967 be amended to add Mitchell Road and Elmwood Drive to Schedule Z (Heavy Traffic).

(B) Proposed Half Load Limit By-law

THAT pursuant to Report TC-GMTOS-2021-02 (Half Load Restrictions within the boundaries of the City of Belleville) THAT the Transportation Committee recommends to Council that a new By-law to designate Half Load Restrictions on Roads Within The Boundaries of the City of Belleville be prepared for Council's consideration and the By-law be given first reading to allow for public input before the By-law is given second and third reading and is finally passed.

Conclusion:

In accordance with the Transportation Committee Terms of Reference, Council approval of the Committee's recommendations is requested.

Respectfully submitted,



Christine Stewart
Deputy City Clerk
On Behalf of the Transportation Committee

CAS/hl

Attachments

- January 27, 2021 Transportation Committee Draft Minutes
- Report TC-GMTOS-2021-01(Mitchell Road & Elmwood Drive – Heavy Truck Traffic Restrictions)
- Report TC-GMTOS-2021-02 (Half Load Restrictions Within the Boundaries of the City of Belleville)



TRANSPORTATION COMMITTEE MINUTES

January 27, 2021

A regular meeting of the Transportation Committee was held at 4:30 p.m. on the above date in a virtual format using the Zoom platform.

ORDER OF BUSINESS

1. CALL TO ORDER

The meeting was called to order.

2. ATTENDANCE

Members Present

Councillor P. Carr
Councillor B. Sandison
Councillor G. Thompson
Councillor R. Williams
Mr. Howard Pulver

Members Absent

Alexandra Cleave

Staff Present

Ms. C. Stewart, Deputy City Clerk
Ms. H. Lloyd, Administrative Assistant
Mr. J. Reid, General Manager Transportation and Operations
Ms. M. Godfrey, Engineer in Training
Mr. S. Whiteman, Manager Outdoor Operations
Mr. S. Ashton, Director of Engineering & Development Services
Sergeant B. Stitt, Belleville Police Service
Sergeant B. Lannin, Belleville Police Service

3. DISCLOSURE OF PECUNIARY INTEREST AND THE GENERAL NATURE THEROF

There were no disclosures at today's meeting.

4. CONFIRMATION OF MINUTES

Minutes of the Committee Meeting held on November 25, 2020.

Moved by Mr. Howard Pulver
Seconded by Councillor Carr

THAT the Minutes of the November 25th, 2020 Transportation Committee Meeting be approved and adopted.

-CARRIED-

5. DEPUTATIONS

There were no deputations at this meeting.

6. REPORTS

6.1 Mitchell Road and Elmwood Drive – Heavy Truck Traffic Restrictions

Moved by Councillor Carr
Seconded by Councillor Sandison

THAT pursuant to Report TC-GMTOS-2021-01 (Mitchell Road and Elmwood Drive – Heavy Truck Traffic Restrictions)

THAT the Transportation Committee recommends to Council that Traffic By-law 12967 be amended to add Mitchell Road and Elmwood Drive to Schedule Z (Heavy Traffic)

-CARRIED-

6.2 Half Load Restrictions within the boundaries of the City of Belleville

Moved by Councillor Sandison
Seconded by Councillor Williams

THAT pursuant to Report TC-GMTOS-2021-02 (Half Load Restrictions within the boundaries of the City of Belleville)

THAT the Transportation Committee recommends to Council that a new By-law to DESIGNATE HALF LOAD RESTRICTIONS ON ROADS WITHIN THE BOUNDARIES OF THE CITY OF BELLEVILLE be prepared for Council's consideration and the By-law be given first reading to allow

for public input before the By-law is given second and third reading and is finally passed.

-CARRIED-

6.3 All-Way stop at the Intersection of Clearview Road and Pine Hill Crescent

Moved by Councillor Williams
Seconded by Councillor Carr

THAT pursuant to the Manager, Outdoor Operation's report (All-way stop at the Intersection of Clearview Road and Pine Hill Crescent-January 18, 2021)
THAT the Transportation Committee recommends to Council that based on warrant analysis, the intersection does not require and All-Way stop.

-CARRIED-

6.4 Roundabout Signage

Moved by Councillor Carr
Seconded by Councillor Williams

THAT pursuant to the Engineer in Training Report (Roundabout Signage – January 27, 2021)
THAT the Transportation Committee recommends to Council that signage that is not in compliance with the Ontario Traffic Manual and Transportation Association of Canada guidelines should not be installed.

-CARRIED-

7. INFORMATION ITEMS

7.1 Regulatory Amendments – Off-road Vehicles

Moved by Councillor Carr
Seconded by Councillor Williams

THAT the information from the Ministry of Transportation regarding changes to the operation of off-road vehicles on Municipal roadways effective January 1, 2021 be received.

-CARRIED-

7.2 Safety Concerns – North Front Street and Donald Street

Moved by Councillor Carr
Seconded by Councillor Williams

THAT the information regarding safety concerns at the intersection of North Front Street and Donald Street be received.

-CARRIED-

7.3 Outstanding Items List – as of January 25, 2021

Moved by Councillor Carr
Seconded by Councillor Williams

THAT the Outstanding Items List as of January 25, 2021 be received.

-CARRIED-

8. NEW BUSINESS

- 8.1** Traffic Calming Pilot Project- M. Godfrey and S. Ashton provided an update regarding the process being followed for traffic calming requests in 2021.
- 8.2** Councillor Williams discussed parking issues along Montgomery Street brought forward by a resident. He requested staff to review and provide a report.

9. NEXT MEETING

The next meeting of the Transportation Committee is scheduled for Wednesday, February 24, 2021 at 4:30 p.m.

10. ADJOURNMENT

The meeting was adjourned at 5:30 p.m.



CITY OF BELLEVILLE
Joseph Reid, General Manager
Transportation and Operations Services
Report No. TC-GMTOS-2021-01
January 27, 2021

To: Transportation Committee

Subject: Mitchell Road and Elmwood Drive - Heavy Truck Traffic Restrictions

Recommendation:

“THAT the Transportation Committee recommend to City Council that By-law 12967 be amended to add Mitchell Road and Elmwood Drive within the Heavy Truck Restriction Appendix”.

Background:

At the City of Belleville Transportation Committee of September 25, 2019 a public request was received requesting to cease Heavy Truck Traffic on Mitchell Road and Casey Road; Item 7.6 and 7.7. Mitchell Road is a surface treated rural road with two at-grade rail crossings and Casey Road is a surface treated road with many businesses along it with a mix of farms and residential. Mitchell Road has become increasingly used by heavy trucks as a shortcut. This has impacted many residents along Mitchell Road, and Elmwood Drive alike, with dust, noise, and safety walking along these rural country roads.

Financial/Analysis:

Both Mitchell Road and Elmwood Drive have CN and CP rail crossings. The CN crossings have been identified by Transport Canada, using the Gradex Crossing Risk rating, as 364 and 472 respectively, out of 22,820 crossings in Canada in their risk assessment. The position of the crossing in relation to the intersections of Airport Parkway is not very conducive for truck crossings.

While the original request was for a restriction on Mitchell Road, staff feel if the route is restricted on Mitchell Road, the trucks will utilize Elmwood Drive instead. We feel it is in the best interest of the community and residents of Elmwood Drive to restrict it as well.

Further, both roads are surface treated, and not designed or engineered for the amount of heavy truck traffic they are receiving. Recently Elmwood Drive and Mitchell Road received new surface treatment south of Airport Parkway. There

are no commercial businesses along Mitchell Road or on Elmwood Drive; the impact to any locally owned business is minimal.

We do not recommend restricting traffic on Casey Road at this time. The restriction proposed on Mitchell Road and Elmwood Drive will help reduce truck traffic on Casey Road. This restriction will have no impact on the businesses on Casey Road, who were concerned with restrictions on that road. City Staff solicited comments from a few businesses on Casey Road.

There are adequate funds within our operating budget to proceed with the installing signage. This restriction will have long term cost savings as it will prolong the life of the road.

Conclusion:

Management is recommending the restriction of Heavy Truck Traffic on Mitchell Road and Elmwood Drive, and leave Casey Road as is for the time being.

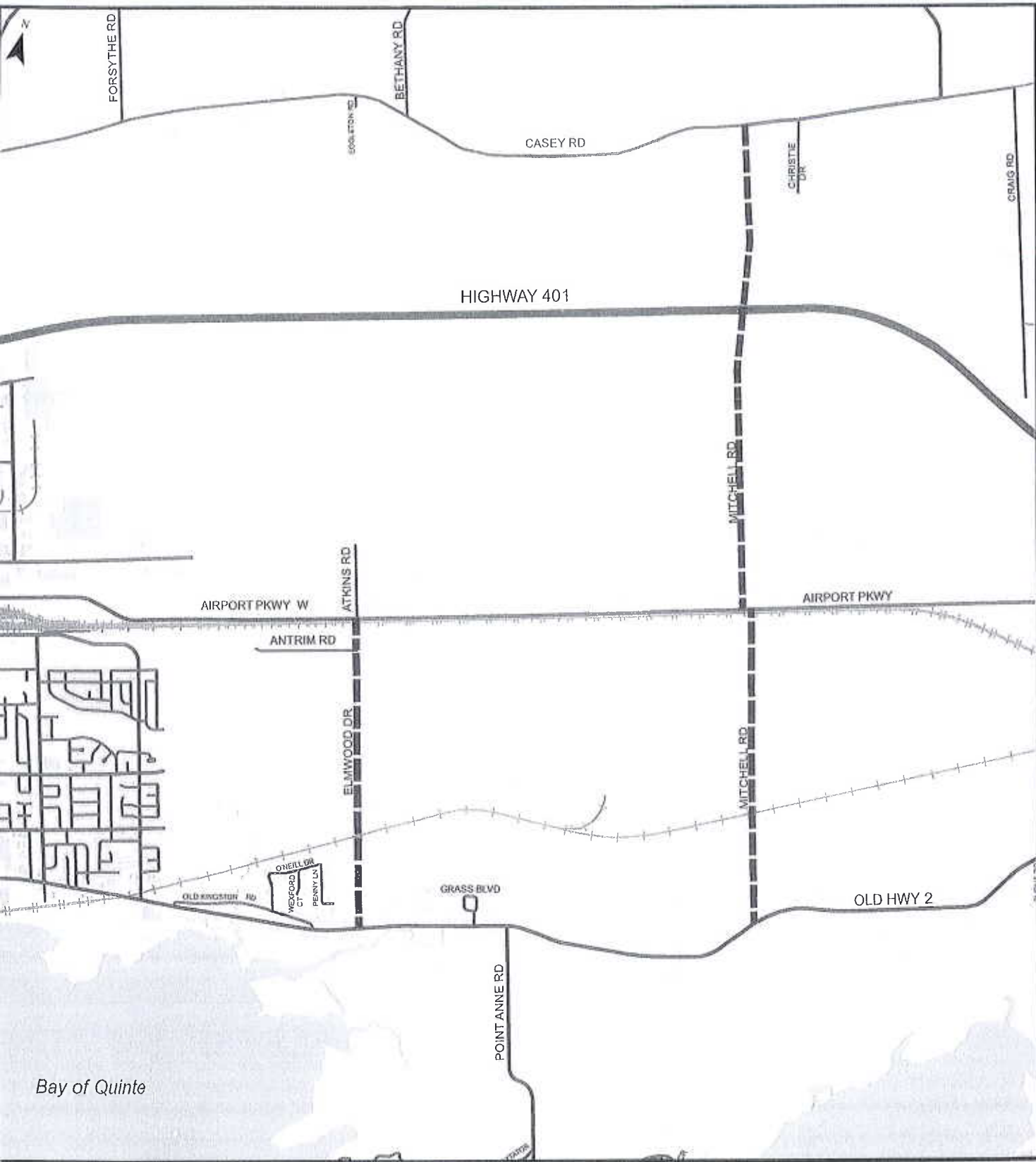
Respectfully Submitted,



Joseph Reid
General Manger
Transportation and Operation Services

Attachments

Schedule 1 - Map



Bay of Quinte



LOCATION MAP



PROPOSED HEAVY TRUCK
RESTRICTIONS

CITY OF BELLEVILLE
ENGINEERING & DEVELOPMENT
SERVICES DEPARTMENT



CITY OF BELLEVILLE
Joseph Reid, General Manager
Transportation and Operations Services
Report No. TC-GMTOS-2021-02
January 27, 2021

To: Transportation Committee

Subject: Half Load Restrictions within the boundaries of the City of Belleville

Recommendation:

"THAT the Transportation Committee recommend to City Council that a new BY-LAW TO DESIGNATE HALF LOAD RESTRICTIONS ON ROADS WITHIN THE BOUNDARIES OF THE CITY OF BELLEVILLE be prepared for Council's consideration and be given only first and second readings, to allow for public input".

Background:

At the City of Belleville Transportation Committee of July 8, 2020 the committee requested staff review the Half-Load Restriction By-law; Item 8.20.

Municipal By-laws relating to Half Load Restrictions are in place as an attempt to preserve and protect municipal roads from damage during the freeze thaw or spring thaw cycle.

Financial/Analysis:

The existing Half Load By-law has been in place since 1998. Over the years, minor issues have been identified which should be addressed with an updated by-law.

The most prominent change is the date in which the roads are posted restricted, and the delegated authority for invoking the restrictions, by the Department Director of Transportation and Operations Services.

The new by-law will allow the delegated authority to implement half load restrictions earlier or extend them later, as the weather requires. Ontario winters can be unpredictable; this change will help protect the city's road network.

The listings of roads within the by-law also require some amendments. One being removing Cloverleaf from the Schedule; this section of Cloverleaf is now known as Millennium Drive.

The identified changes will have no negative financial impact on budgets; they should have long term cost savings in further prolonging the life of the roads.

Conclusion:

Management is recommending that By-law Number 98-23, being a by-law to designate half load restrictions on roads within the boundaries of the City of Belleville, and By-law Number 2003-35, being a by-law to amend by-law 98-23, be rescinded and the new by-law be prepared for Council's consideration and be given only first and second readings, to allow for public input.

Respectfully Submitted,



Joseph Reid
General Manger
Transportation and Operations Services

Attachments

- Schedule 1 – By-law Number 98-23
- Schedule 2 – By-law Number 2003-35
- Schedule 3 - Proposed By-law

CORPORATION OF THE CITY OF BELLEVILLE
BY-LAW NUMBER 2021-XX

A BY-LAW TO DESIGNATE HALF LOAD RESTRICTIONS ON ROADS WITHIN THE BOUNDARIES OF THE CITY OF BELLEVILLE

WHEREAS, Subsection (7) of Section 122 of the Highway Traffic Act, R.S.O. 1990, Chapter H.8., as amended provides that the Municipal Corporation or other authority having jurisdiction over a highway may by by-law designate the date on which a reduced load period shall start or end and the highway or portion thereof under its jurisdiction to which the designation applies;

AND WHEREAS, the reduced load period is deemed necessary for the protection of certain highways in the Corporation of the City of Belleville.

NOW, THEREFORE, THE COUNCIL OF THE CORPORATION OF THE CITY OF BELLEVILLE ENACTS AS FOLLOWS:

1. THAT the provisions of Subsection 1., 2., 3. and 4. of Section 122 of the Highway Traffic Act, apply to roads within the boundaries of the former Township of Thurlow Ward now known as WARD 2 in the City of Belleville during the period from 15th day of February to the 30th day of April, inclusive, in each and every year.
2. THAT the only portions of roads exempted from the provisions of paragraph 1. of this by-law are:
 - (i) Casey Road from Highway #37 easterly to Forsythe Road;
 - (ii) Putman Road and part of unopened road allowance between Concession 2 and Concession 3 and extending easterly to the southwest limit of Lot #1, Registered Plan Number 53 and shall include Part 5, Plan Number 21R-15284 and Part 1,2,3,4 and 5, Plan Number 21R-16211;
 - (iii) Millennium Drive
 - (iv) Blessington Road from Highway #37 easterly 800 metres.
 - (v) Mineral Road;
 - (vi) Parks Drive;
 - (vii) Maitland Drive from Highway #62 easterly to Cannifton Road North;
 - (viii) Tank Farm Road;

- (ix) Cannifton Road North from River Road easterly to Highway #37;
- (x) River Road from Cannifton Road North to former Corby Distilleries Main Plant entrance;
- (xi) Ashley Street in the Hamlet of Foxboro.

3. The Department Head of Transportation and Operations Services, or delegate, upon written notification to the CAO and Council, has the authority to amend these yearly dates as required by road and climatic conditions.

4. This by-law shall not become effective until appropriate signs have been erected and are on display.

That City of Belleville By-law 98-23 and all amendments, and By-Law Number 3946 of the former Township of Thurlow hereby rescinded in its entirety.

THIS BY-LAW SHALL COME INTO FORCE AND TAKE EFFECT IMMEDIATELY ON AND AFTER THE PASSING THEREOF.

Read a first time this XXth day of February 2021.

Read a second time this XXth day of February 2021.

Read a third time and finally passed this XXth day of February 2021.

THE CORPORATION OF THE CITY OF BELLEVILLE

BY-LAW NUMBER 2003-35

A BY-LAW TO AMEND BY-LAW 98-23 BEING A BY-LAW TO DESIGNATE HALF LOAD RESTRICTIONS ON ROADS WITHIN THE BOUNDARIES OF THE CITY OF BELLEVILLE

WHEREAS, Subsection (7) of Section 122 of the Highway Traffic Act, R.S.O. 1990, Chapter H.8., as amended provides that the Municipal Corporation or other authority having jurisdiction over a highway may by by-law designate the date on which a reduced load period shall start or end and the highway or portion thereof under its jurisdiction to which the designation applies;

AND WHEREAS, the reduced load period is deemed necessary for the protection of certain highways in the Corporation of the City of Belleville.

NOW, THEREFORE, THE COUNCIL OF THE CORPORATION OF THE CITY OF BELLEVILLE ENACTS AS FOLLOWS:

1. THAT Section 2 of By-Law Number 98-23 be amended to include the following:
 - (xiii) Blessington Road from Highway #37 easterly 800 metres.
2. THAT By-Law Number 3946 of the former Township of Thurlow is hereby rescinded in its entirety.

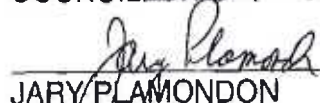
THIS BY-LAW SHALL COME INTO FORCE AND TAKE EFFECT IMMEDIATELY ON AND AFTER THE PASSING THEREOF.

Read a first time this 24th day of **February 2003**.

Read a second time this 24th day of **February 2003**.

Read a third time and finally passed this 24th day of **February 2003**.


COUNCILLOR CLIFF BELCH, ACTING MAYOR


JARY PLAMONDON CITY CLERK

THE CORPORATION OF THE CITY OF BELLEVILLE

BY-LAW NUMBER 98-23

A BY-LAW TO DESIGNATE HALF LOAD RESTRICTIONS ON ROADS WITHIN THE BOUNDARIES OF THE CITY OF BELLEVILLE

WHEREAS, Subsection (7) of Section 122 of the Highway Traffic Act, R.S.O. 1990, Chapter H.8., as amended provides that the Municipal Corporation or other authority having jurisdiction over a highway may by by-law designate the date on which a reduced load period shall start or end and the highway or portion thereof under its jurisdiction to which the designation applies;

AND WHEREAS, the reduced load period is deemed necessary for the protection of certain highways in the Corporation of the City of Belleville.

NOW, THEREFORE, THE COUNCIL OF THE CORPORATION OF THE CITY OF BELLEVILLE ENACTS AS FOLLOWS:

1. THAT the provisions of Subsection 1., 2., 3. and 4. of Section 122 of the Highway Traffic Act, apply to roads within the boundaries of the Thurlow Ward area of the City of Belleville during the period from 1st day of March to the 30th day of April, inclusive, in each and every year.
2. THAT the only portions of roads exempted from the provisions of paragraph 1. of this by-law are:
 - (i) Casey Road from Highway #37 easterly to Forsythe Road;
 - (ii) Putman Road and part of unopened road allowance between Concession 2 and Concession 3 and extending easterly to the southwest limit of Lot #1, Registered Plan Number 53 and shall include Part 5, Plan Number 21R-15284 and Part 1,2,3,4 and 5, Plan Number 21R-16211;
 - (iii) Cloverleaf Drive from Highway #62 easterly to Mineral Road;
 - (iv) Mineral Road from Cloverleaf Drive to Maitland Drive;
 - (v) Parks Drive from Mineral Road to Maitland Drive;

- (vi) Maitland Drive from Highway #62 easterly to Cannifton Road North;
- (vii) Cannifton Road North from Maitland Drive easterly to Tank Farm Road;
- (viii) Tank Farm Road from Cannifton Road North to Highway #37;
- (ix) Cannifton Road North from River Road easterly to Highway #37;
- (x) River Road from Cannifton Road North to Corby Distilleries Main Plant entrance;
- (xi) Wisers Road from the intersection of Highway #37 southerly to the Corby Distilleries Main Plant entrance;
- (xii) Ashley Street in Foxboro from Highway #62 northerly to Old Madoc Road.

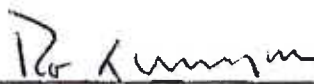
3. THAT By-Law Number 3946 of the former Township of Thurlow is hereby rescinded in its entirety.

THIS BY-LAW SHALL COME INTO FORCE AND TAKE EFFECT IMMEDIATELY ON AND AFTER THE PASSING THEREOF.

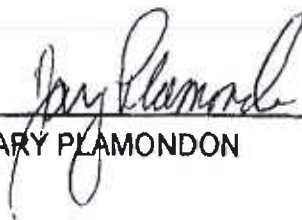
Read a first time this 23rd day of February, 1998.

Read a second time this 23rd day of February, 1998.

Read a third time and finally passed this 23rd day of February, 1998.



ROSS L. MCDOUGALL MAYOR



JARY PLAMONDON CITY CLERK



CITY OF BELLEVILLE
Maria Godfrey, Engineer in Training
Engineering and Development Services
Report No. ENG-2021-01
February 8, 2021

APPROVAL BLOCK	
CAO	<u> PAB </u>
DEDS	<u> SA </u>
ME	<u> RT </u>

To: Mayor and Members of Council

Subject: Sidewalk Patio Standards and Application Guidelines

Recommendation:

“THAT the Sidewalk Patio Standards and Application Guidelines be endorsed by City Council.”

Strategic Plan Alignment:

The City of Belleville’s Strategic Plan identifies nine strategic themes. The recommendation within this report aligns with one (1) of the City’s strategic themes, “City Centre Revitalization” and the City’s strategic objective to “Encourage the creation of a vibrant downtown, accented with pedestrian-friendly services and unique residential and commercial opportunities”.

Background:

For a number of years, the City has been approving sidewalk patios for businesses in the downtown district however there were never any guidelines other than maintaining a minimum 1.5m pedestrian pathway. The provincial and local health unit restrictions to indoor dining for restaurants in 2020, created a challenge for restaurants in the City’s downtown and outdoor dining alternatives became a priority. In an effort to support these businesses, City staff and Council expedited the existing process for approving sidewalk patios through the use of encroachment agreements by making the review of these applications a priority for staff and delegating approval authority to the Director of Engineering and Development Services.

Given the increase in demand and the lack a formal policy or clear guidelines regarding sidewalk patios, the need for a formal policy or guidelines was identified as a priority to provide clear guidance to business owners.

Financial/Analysis:

Staff initiated the process of developing a formal policy or clear guidelines to assist business owners in developing sidewalk patios and to ensure accessibility requirements and other municipal priorities are addressed.

To aid in the development of a formal policy or clear guidelines, staff undertook a review of sidewalk patio guidelines in use by other municipalities, consulted with the City's Accessibility Co-ordinator, the City's Accessibility Committee and the Belleville Downtown District BIA .

The following quote, supporting the proposed guidelines, was provided by the executive director of the Belleville Downtown District BIA;

"The new patio standards guidelines are a necessary tool for our membership to be able to plan patio configurations with flexibility and within acceptable setup for accessibility. This will greatly improve planning and execution of patios, and lessen confusion going forward." – Marijo Cuerrier, Belleville Downtown District BIA

The proposed Sidewalk Patio Standards and Application Guidelines provides four options for patios and direction to ensure uniformity for all sidewalk patios in the City. The standards and guidelines also include a maintenance and enforcement section to ensure compliance.

As a result of the process used, staff is confident that the proposed Sidewalk Patio Standards and Application Guidelines will assist business owners and provide more efficient approvals to address outdoor dining alternatives for City businesses.

There are currently seven businesses in the City that have an existing encroachment agreement with the City. These businesses are as follows;

- Sans Souci Food Services Limited
- Melanie Hilmi and Maxwell Paper Canada Inc.
- James Alexander Bruce, Celine LePage and Sean McKinney
- Chilangos Mexican Restaurant and Maury Flunder
- L'Auberge De France Limited
- Dinkel's Restaurant and Café and Notyal Enterprises Limited
- Park Provisioners Barbershop & Haberdashery and Clothbuilders Holdings

It is recommended by staff that these existing agreements are honoured but updated with a new patio application, excluding the associated fees, to comply with the design criteria in the proposed Sidewalk Patio Standards and Application Guidelines.

Conclusion:

It is recommended that the Sidewalk Patio Standards and Application Guidelines be adopted by City Council.

Respectfully submitted,

A handwritten signature in black ink, appearing to read "M. Godfrey". The signature is written in a cursive, flowing style.

**Maria Godfrey,
Engineer in Training, Engineering**

Attachment 1 – Patio Encroachment Guidelines



Sidewalk Patio Standards and Application Guide

Engineering and Development Services Department



Contents

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The City of Belleville would like to thank the City of London for allowing us to use some of their graphics to assist us with the various patio set-up classifications and design details.

1.0 Purpose

To **clearly state our expectations** for seasonal patios and other pop-ups that will occupy part of our municipal sidewalk, on-street parking spaces, or both.

Our goal is to support property and business owners to successfully obtain an encroachment agreement by outlining the specific requirements and minimum standards.

Sidewalk patios and other pop-ups can help create a lively urban environment. Our intention is to ensure that any developments uphold and enhance universal accessibility, public safety, and our desired vision for the streetscape.

We want to encourage **inclusive use and enjoyment** of our local businesses and community spaces.

The standards in this guide apply to all businesses and property owners in Belleville who wish to place a seasonal patio or other pop-up within the public right-of-way, in on-street parking spaces, or both.

We invite you to consult this guide prior to completing your application for an encroachment agreement. To get an encroachment agreement you must complete the application and consultation process with our Engineering and Development Services Department and pay the associated fees.



2.0 Application Process

2.1 General Overview

The City will approve seasonal patios and other pop-ups that will occupy a municipal sidewalk, an on-street parking space, or both through an encroachment agreement.

- Encroachment agreements are valid seasonally from May 1st to October 31st.

Our Engineering and Development Services (EDS) Department handles applications and encroachment agreements. You can complete your application by:

- picking up a copy from EDS on the second floor of City Hall, or
- download the [Encroachment Agreement Application](#) from our website

Once you complete your application submit it to EDS along with the required fees.

The first time you apply for an encroachment agreement you will need to submit the full application and all supporting materials.

After your initial encroachment agreement, each subsequent year you only need to submit a renewal application, unless anything changes or if there are any issues.

We have the right to reject an application if we determine that the patio or pop-up does not enhance our public spaces or comply with our minimum standards.

All seasonal patios and pop-ups must comply with the Accessibility for Ontarians with Disabilities Act and support inclusive use and enjoyment of our public spaces.



2.2 Steps in the Application Process

1. Select Patio Classification

Use Section 3 of this guide to determine which patio set-up is best suited for your location.

Note: If you will be using an area in front of an adjacent property for your set-up you need to include either:

- written consent from that property owner, or
- if the usage is in support of a Belleville Downtown Improvement Area (BDIA) initiative, a letter of support from the BDIA

There are six patio classifications that show acceptable set-ups. **Select an approved classification from Section 3** and then **follow the design standards from Section 4** to ensure your patio or pop-up complies with the minimum standards.

- Where standards are enhanced, meet the desired outcomes of these guidelines.

2. Plans & Drawings

We only require these plans and drawings for your initial application. If the details of your patio or pop-up stay the same each year, after you get your first encroachment agreement you only need to submit a renewal application.

We may require you to submit another full application with new plans and drawings if:

- you are redesigning or resizing the patio, or
- there were issues noted with your set-up during the previous season

When completing your initial application, you must include plans or drawings that show:

- a. An overhead view of the set-up and location of your patio or pop-up
 - It must show compliance with the classification and design requirements, with obvious indication of your 1.5 metre clear width for path of travel
- b. A street-level view of your proposed design features such as fencing, flowers and planters, furniture, and lighting for example
 - Include any sample photos, drawings, or plans that provide details about your design features
- c. The entrance to the patio and entrance to your building

- d. The location of any surrounding streetscape amenities, such as trees, bike racks, traffic partitions, and benches for example
- e. Any adjacent properties that may be affected by your set-up
- f. Details of the patio surface and how elements will be affixed to the surface
- g. Include at least three photos of the patio or pop-up area including one from the front facing your business, one from the left, and one from the right

3. Submit for Staff Review

Submit your package to EDS staff on the second floor of City Hall. Staff from various City of Belleville departments may be involved in the review of your application and will provide feedback on modifications or recommend it for approval.

To determine approval, we will evaluate your application based on the following criteria:

- Compliance with the minimum standards outlined in this guide
- Accessibility, compliance with Accessibility for Ontarians with Disabilities Act and assessment of inclusive use
- Safety of the patio location, set-up, and design features
- Structural compliance
- Design, how well your chosen features fit with our desired streetscape and enhance the public space

4. Consultation

In some cases, after reviewing your application we may call you in for a consultation. During this consultation we will discuss details specific to your location and desired set-up, and how to ensure your application meets the requirements.

5. Construct and Operate

Once approved, we will issue you an encroachment agreement to proceed with your plans as outlined in your application.



3.0 Classifications



There are six potential set-ups for a patio. Selecting the right classification for your specific location will ensure the streetscape is enhanced and the safety of patrons and the public is secured.

- When choosing your classification, **remember to consider** the existing streetscape, surrounding uses, and availability of space.

Patios must respect adjacent businesses and not impede the flow pedestrians or pedestrian traffic movements.

Sidewalk patios fit into two categories:

1. Boulevard patios, set-ups entirely within the public right of way (sidewalk)
2. On-street parking space patios, set-ups using an on-street parking space

The following pages explain in detail each of the six potential patio locations and include minimum and maximum measurements related to each situation.

3.1 Boulevard Patios

1. Along the Curb

- Located on the municipal sidewalk, along the edge of the curb.
- Clear path of travel is located between the associated building and the inside edge of the patio.
- Ensure there is a clear 1.5m radius around streetscape objects that are within the path of travel.
- Clear path of travel is measured from the edge of tree grates, light posts, and any fixed elements.
- Minimum buffer of 0.5m between the outside edge of the patio and the curb.
- Align the patio entrance with the main entrance of the business.

We prefer this set-up as it maintains a consistent path of travel along the storefronts for pedestrians, while still providing adequate patio space.

Summary of Required Distances

Clear Path of Travel	1.5m (min)
Distance from Curb	0.5m (min)
Entrance	1.2m (min)
Distance from Streetscape Objects	1.5m (min)

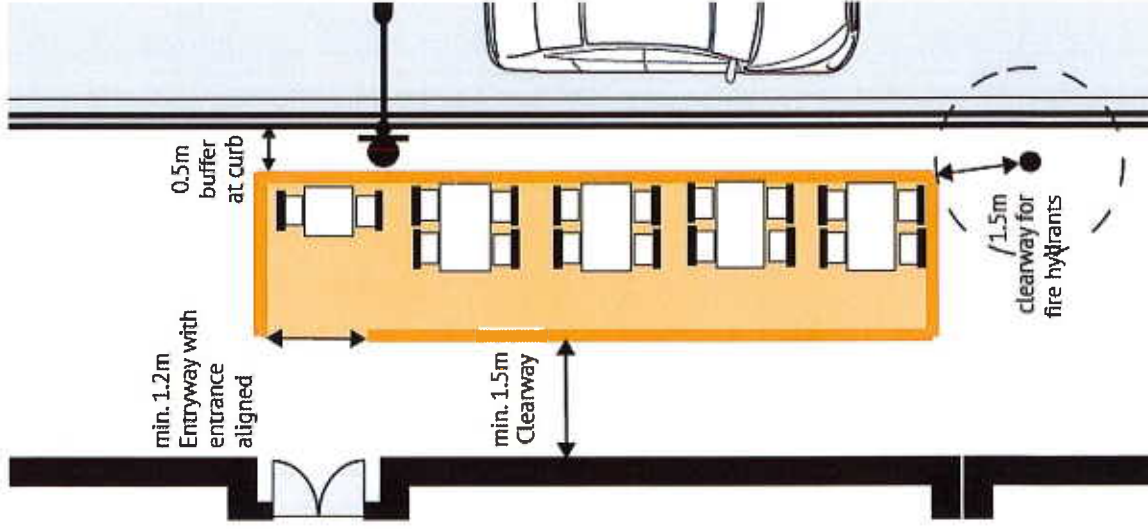


Image Credit: City of London, ON

2. Along the Building

- Located right along the building and extending out beyond the front property line into the sidewalk area.
- Clear path of travel is located between the outer edge of the patio and the curb.
- Ensure there is a clear 1.5m radius around streetscape objects that are within the path of travel.
- Clear path of travel is measured from the edge of tree grates, light posts, and any fixed elements.
- Align patio entrance with the main entrance of the business.

Note: This classification is unsuitable for locations where there are a lot of existing streetscape objects that would cause a zig-zag path of travel for pedestrians. In such situations, where patios can be located elsewhere this set-up should not be considered.

We will assess this when reviewing your application.

Summary of Required Distances

Clear Path of Travel	1.5m (min)
Entrance	1.2m (min)
Distance from Streetscape Objects	1.5m (min)

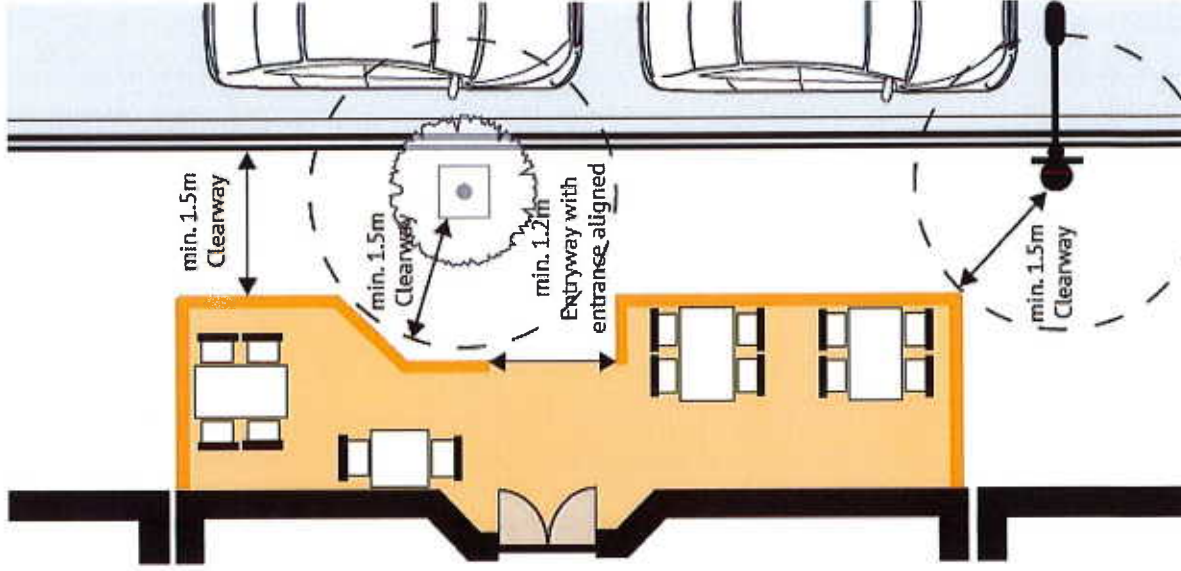


Image Credit: City of London, ON

3. Near a Corner

Patio is located along the curb and follows the same set-up requirements as #1 OR patio is located along the building and follows the same set-up requirements as #2.

Patios near corners and intersections have additional requirements to ensure the increased activity will not interfere with pedestrian flow and safety.

- Minimum clearway of 3m out from the corner and intersection.
- Maintain a 6m x 6m sight triangle across the corner.

Summary of Required Distances

Clear Path of Travel	1.5m (min)
Distance from Curb	0.5m (min)
Clearway from Corner	3.0m (min)
Entrance	1.2m (min)
Distance from Streetscape Objects	1.5m (min)

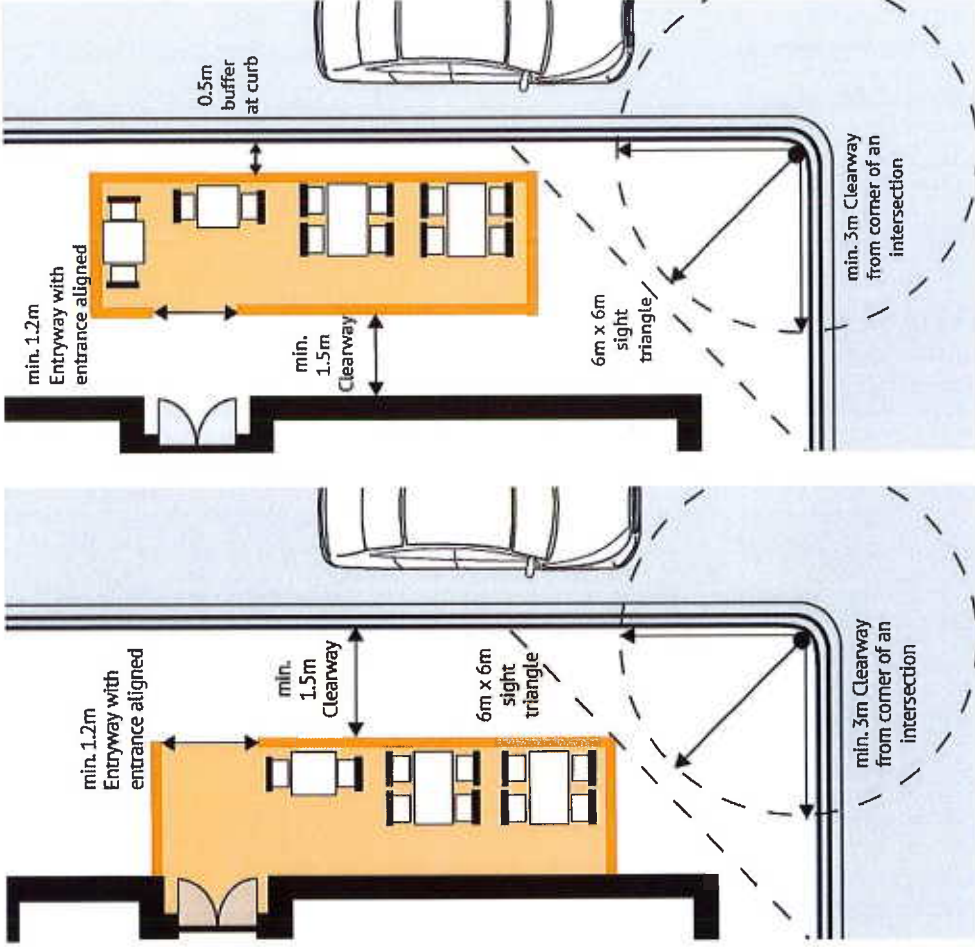


Image Credit: City of London, ON

3.2 On-street Parking Space Patios

4. In On-Street Parking Space

Where the public right-of-way is not large enough to support a patio on the sidewalk, or subject to City consideration, patios may be located within an on-street parking space subject to an agreement with the City.

- Regular sidewalk is maintained, clear path of travel is located between the associated building and the curb.
- Patio surface must be brought to the same height as the sidewalk and have no gap between surfaces.
- Patios must occupy complete parking spaces and have a minimum of 4m of the parking space located directly in front of the associated business.

This set-up has a minimal effect on the flow of pedestrian traffic and will be preferred in some locations. We will assess this when reviewing your application.

Summary of Required Distances

Clear Path of Travel	1.5m (min)
Length	6.7m (min) (1 parking space)
Entrance	1.2m (min)
Distance from Streetscape Objects	1.5m (min)

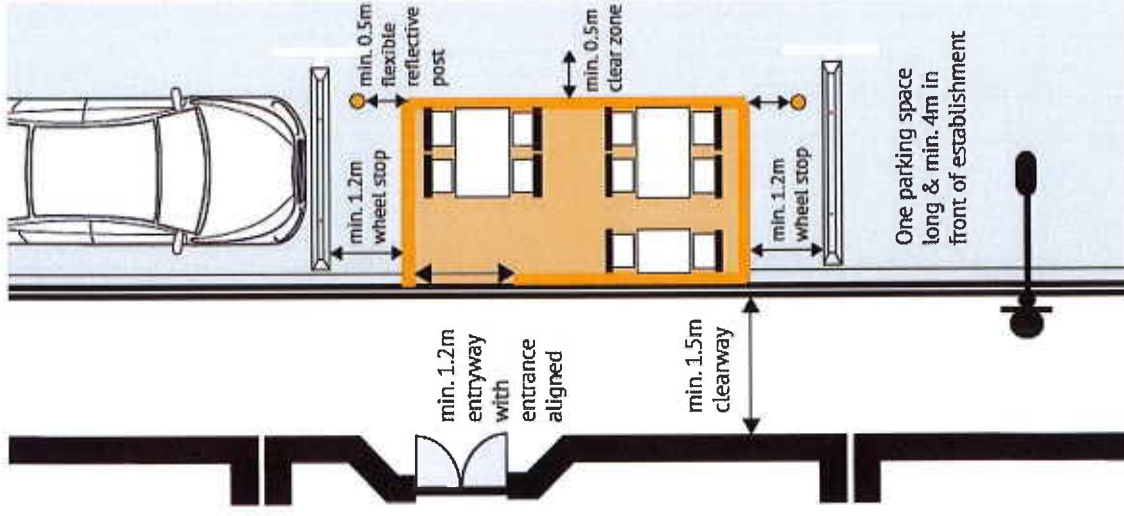


Image Credit: City of London, ON

5. Joined Combination

To create a larger patio, it may be suitable to choose a set-up that combines sidewalk space and on-street parking.

- Clear path of travel is located between the inside edge of the patio and the associated building.
- Ensure there is a clear 1.5m radius around streetscape objects that are within the path of travel.
- Use of an on-street parking space is subject to an agreement with the City.
- Align the patio entrance with the main entrance of the business.
- Patio surface must be firm, flat, and stable. The surface within the on-street parking space must be brought to the same height as the sidewalk. There must be a level transition with no gap between the two portions.

We prefer this option because it allows a larger patio to accommodate more patrons, while maintaining a consistent clear path of travel along the storefronts for pedestrians.

Summary of Required Distances

Clear Path of Travel	1.5m (min)
Length	6.7m (min) (1 parking space)
Entrance	1.2m (min)
Distance from Streetscape Objects	1.5m (min)

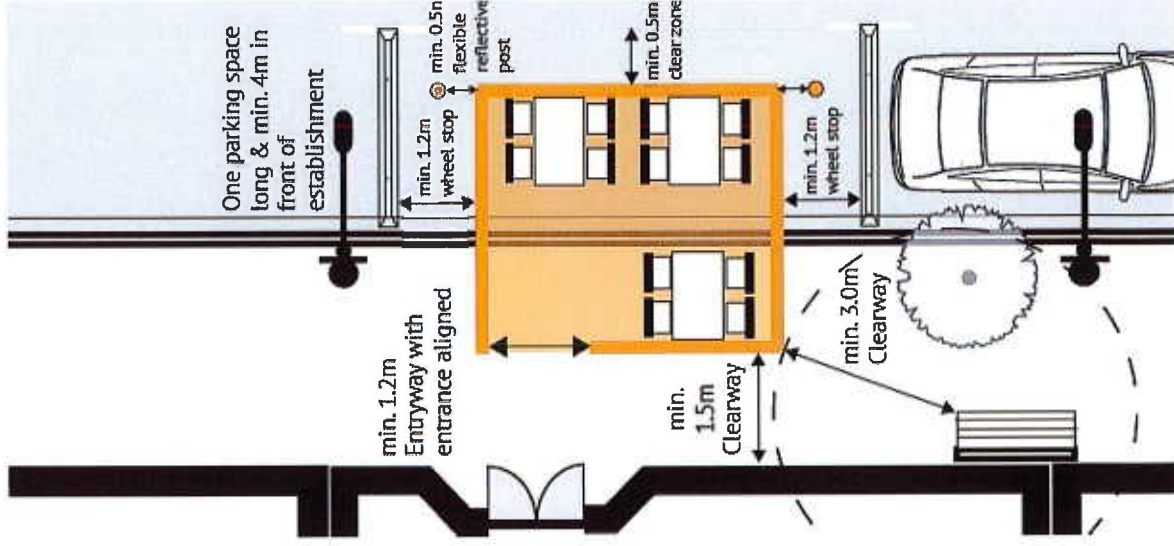


Image Credit: City of London, ON

6. Split Combination

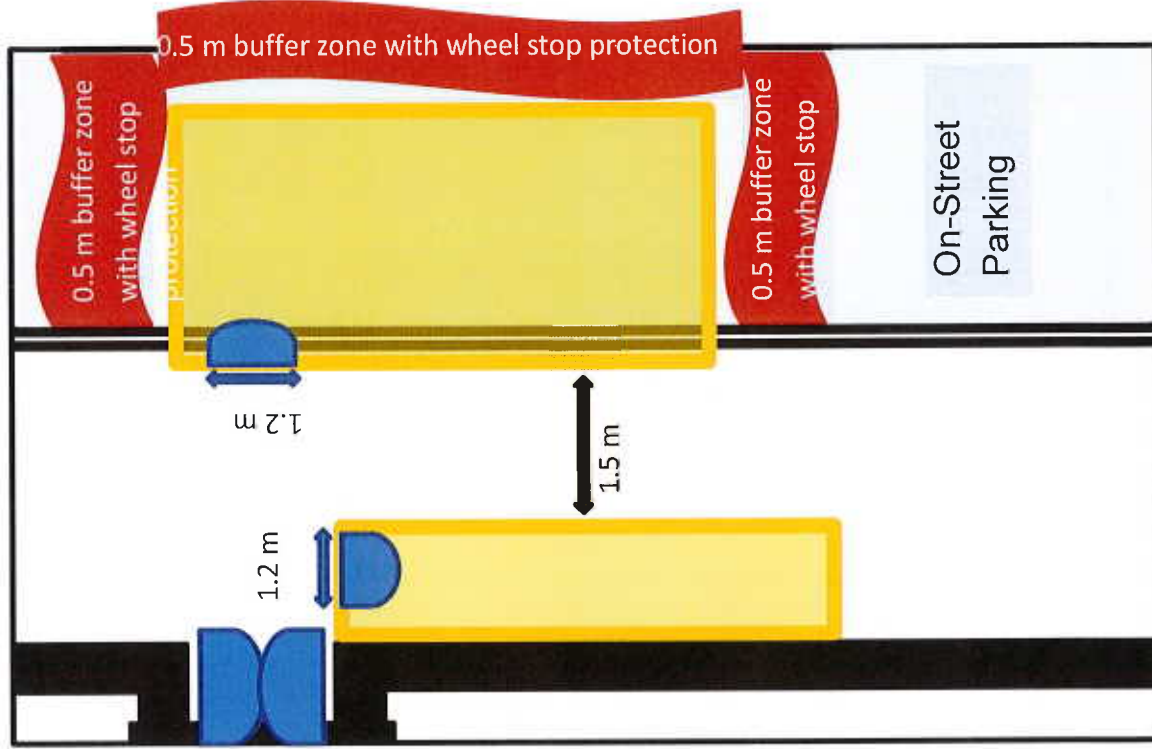
Depending on your location and the existing streetscape objects, it may be suitable to choose a split combination to gain a larger patio space.

- Patio space within the on-street parking spot must meet the same set-up requirements as #4.
- Patio space along the building must meet the same set-up requirements at #2, with the patio entrance located as close as possible to the main business entrance.
- Clear path of travel is located between the inside edge of the patio space that is in the on-street parking spot and the inside edge of the patio space that is along the building.
- Use of an on-street parking space is subject to an agreement with the City.

This option also allows for a larger patio to accommodate more patrons, while still maintaining a relatively consistent clear path of travel for pedestrians. This option should be considered based on your location and the existing streetscape objects in the area.

Summary of Required Distances

Clear Path of Travel	1.5m (min)
Length	6.7m (min) (1 parking space)
Entrance	1.2m (min)
Distance from Streetscape Objects	1.5m (min)



3.3 On-Street Parking Considerations

- A full parking space length must be used. The parking area that remains available on either side of the patio must provide one or more full parking spaces of 6.7m length.
- The outside edge of the patio must be at least 0.5m away from a lane of traffic, creating a 0.5m buffer zone.
- Wheel stops must be placed 1.2m from the side of a patio to provide a buffer zone between it and the next parking spot.
- Patios must have flexible reflective posts, at least 1.2m high, placed 0.5m from both corners of the patio that are in the roadway. The reflective elements should be in line with the outside edge of the patio that is parallel to the lane of traffic.
- You are responsible to pay a fee for the use of the on-street parking space., Fees are calculated based on the average revenue generated by an on-street parking space during the term or season.
- The patio surface must be designed for easy removal and to prevent damage to the underlying roadbed. Your plan for installing wheel stops and flexible posts must be included as part of your application and is subject to our approval.
- The substructure design of a patio will vary and depends on the slope of the street and the overall design of the structure. The sub-structure must accommodate the crown of the road and provide a level surface for the patio. Adjustable deck support pedestals that can be placed under the surface and of different heights are a common application. Another method is to provide steel sub-structure and angled beams.
- The design of the patio structure should not inhibit the adequate drainage of storm water runoff. Small channels between the base and the platform should facilitate drainage.
- Guardrails must be present around the three sides of the patio that border parking spots and traffic laneways. They must be constructed to the following specifications:
 - minimum 1.07m high
 - designed and attached in a manner to resist a concentrated load at any point of not less than 1kN
 - designed and attached in a manner to resist a uniformly distributed load of 0.75kN / m
- Open guardrails are encouraged to reduce the risk of high winds using the guardrail to move the patio structure
 - Openings through the guardrail shall be a size that will prevent the passage of a spherical object having a diameter of 100mm.

4.0 Design Details

Sidewalk patios consist of structural, functional, and decorative features. To ensure public safety, inclusive use, and enhancement of the vision for our streetscapes, the following sections outline our minimum requirements for the various features of a sidewalk patio.

We encourage creativity and the development of unique outdoor dining spaces that meet our design detail requirements.

4.1 Functionality

Clearway

- Maintain an unobstructed pedestrian pathway of at least 1.5m at all times. The location of the path of travel relative to the patio will depend on your patio set-up.
- Clear path of travel is measured from the edge of tree grates, light posts, and any fixed elements.
- Patios located at a corner will require a clearway of 3m out from the corner of the intersection.

Entryway

- Provide an entryway to the patio of at least 1.2m wide and, where possible, align it with the entrance to the corresponding business.
 - If the patio entrance cannot align with the business entrance they should be as close to each other as possible.
- Maintain the clear 1.2m wide entryway at all times. Signage, A-frame boards, host / hostess stands, and other objects cannot impede this 1.2m clearance.
- Patio design and set up must not interfere with any existing barrier-free entrance or accessibility features of your building

- 1.5 m clear radius around power door operators
- 1.2 m clear width that allows a forward approach

Streetscape Elements

- There must be a 1.5m clear radius around streetscape objects such as garbage receptacles, tree grates, fire hydrants, street furniture, and any other item that poses an obstruction within the clear path of travel.
- Having streetscape elements within the path or travel along your patio is acceptable as long as the 1.5 clear width is maintained, and they do not cause pedestrians to zig-zag around them.
- Fire hydrants require a 1.5m buffer zone at all times.
- Streetscape elements, outside of the clear path of travel do not require a buffer. For example, a tree grate can be near the edge of your patio if it is not within the path of travel.
- Street trees may be incorporated into the patio

Seasonal Use

- Sidewalk patios, in their entirety, may only operate as seasonal features. All elements must be removable and disassembled at the end of the season, October 31.

Consistency

- Tables and chairs should be made of matching sets of themes and be consistent throughout the patio.

Materials

- All furniture should be made of durable, weather resistant materials that are easily cleaned.
- Composite, metal or painted, pressure treated, and stained wood is preferred for decking and fencing.
- Deck surfacing must be firm, flat and stable, and have a non-slip surface.

4.2 Furniture

Host Stands

- Host stands are permitted within the patio provided they are consistent with the furniture in the patio and do not enter the clearway.

Garbage Receptacles

- Garbage receptacles are permitted within the patio provided they are located with host stands and are not a prominent feature within the space.

Menus & Sandwich Boards

- Menus and sandwich boards are permitted, provided they complement the patio and do not enter the clearway.
- Menus and sandwich boards may not extend beyond 1m high from the ground and cannot obstruct views

Tables

- Design must allow for at least one table that is accessible to someone seated in a mobility device. Consider the following table design features:
 - Knee and toe clearance
 - Forward approach
 - Turning radius
 - Transfer option

4.3 Fences and Plantings

Fences

- Fencing is always required, regardless of liquor license and set-up classification
- Patio fencing must be 0.75 - 1m high
- Fencing must have a solid continuous line that would allow a pedestrian to follow along the edge of the patio to navigate around it
- Fencing used as a guardrail must abide by the On-street Parking Considerations in Section 3.3

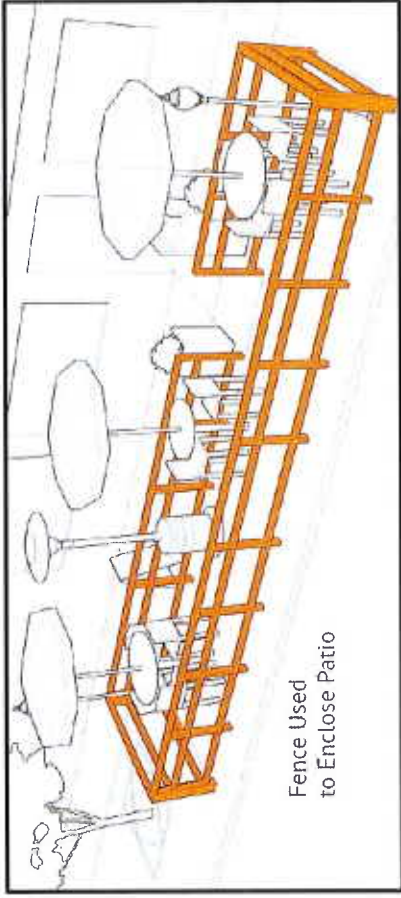


Image Credit: City of London, ON

Plant Materials

- Plants must be healthy, living, and maintain throughout the season

Planter Fence

- Planters can be used in lieu of a fence but must be adequately sized to define the patio space.
- The planter with plants must be 0.75 - 1m high and a continuous length to delineate the patio space.
- Planters must maintain a 0.5m buffer from the curb.

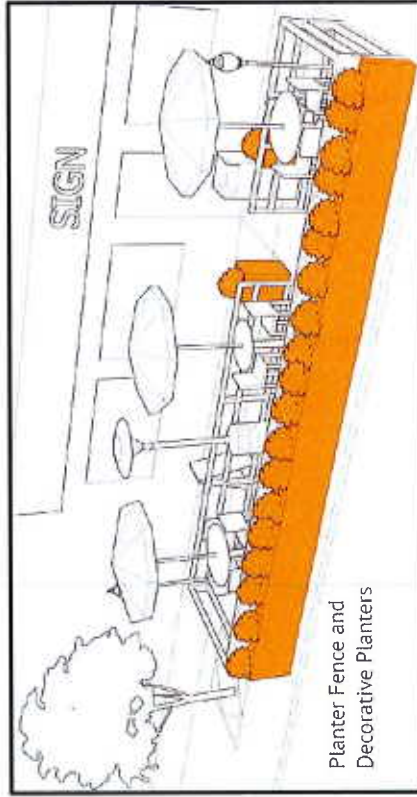


Image Credit: City of London, ON

Decorative Planters

- Ground planters may be used within the patio for decoration. The planters may not exceed 0.5m wide and 1m high. Planters with plant material may not exceed 1.5m high and cannot interfere with sight lines.
- Hanging planters and acceptable. Plant material may extend a maximum of 0.5m from the hanging planter box.

4.4 Additional Features

Awnings & Umbrellas

- Awnings and umbrellas must have a minimum height of 2.4m. Awnings must be installed on the first storey of the building.
- Any awnings and umbrellas made of fabric must be flame and fade resistant material.

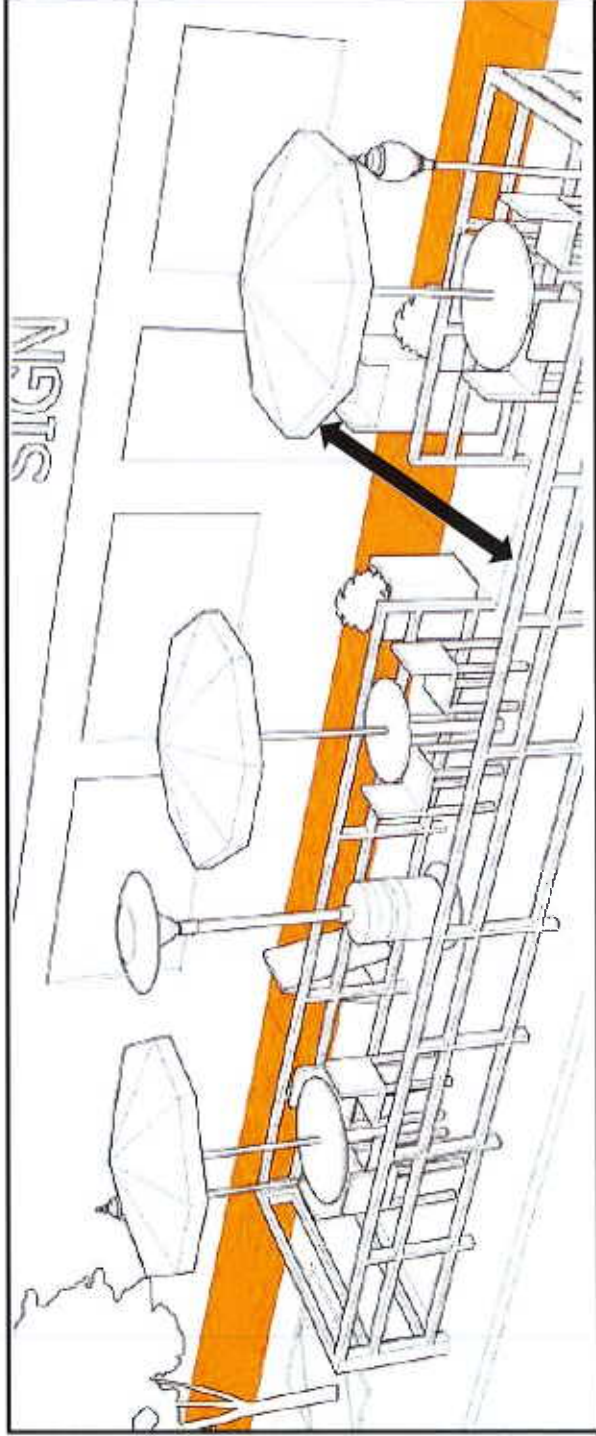


Image Credit: City of London, ON

Heaters

- Heaters are permitted within the patio provided that:
 - they do not exceed 3m in height
 - are used within the manufacturer's guidelines
 - you follow all installation, operation, and safety instructions
 - the units are not in an entryway or exit
- the units are fastened to the surface they are on or at a minimum equipped with a tip over safety switch
- the units are clearly labelled "HOT do NOT touch"
- the installation and operation manuals are kept on site and available to the "Authorities Having Jurisdiction" (AHJ)

- Lighting for the patio is permitted, provided it is within the boundaries of the patio and does not infringe the clearways.
- Lights may not exceed 3m in height. The brightness of the lights should be sensitive to the uses surrounding the patio and should be directed onto the patio, away from neighbouring properties and the street.
- Lighting is restricted to low voltage (12V system), 120VAC is not permitted.

Paving

- Site specific paving or surface treatment is not permitted in the public right-of-way.
- Where paving surface is required, such as to level an on-street parking space to the sidewalk, the material should be strong, durable, complimentary to the existing paving materials, and removable at the end of the season.



5.0 Maintenance and Enforcement

The business owner is expected to maintain the sidewalk patio area and furniture in a clean and hygienic manner, and secure or store the patio furniture when not in use outside the hours of business operation. At the termination of the sidewalk patio permit, you must return the patio area to its pre-approved condition.

Businesses that do not comply with the standards and requirements of this guide will first be given a warning detailing the issues and time to fix the problem. If the problem persists, your encroachment agreement may be terminated and the City may take steps to rectify the problem at your expense.

We reserve authority over the road right-of-way and, when required, will exercise the right to request removal of sidewalk patio areas due to safety, construction, or community needs.

If we have any safety objections, the sidewalk patio will not be approved or we will issue notice of removal within 24 hours when conflicts are identified. Emergencies may necessitate faster removal times as required by the City or emergency Services. Under these situations, care will be taken for removal of infrastructure, the City will not be held liable for damages to sidewalk patio infrastructure added to the road right-of-way by the proprietor.

The permit holder is required to remove (at their own expense) their patio during public works projects such as repaving and sidewalk widening.



APPROVAL BLOCK	
CAO	<u>RB</u>
DRCCS	<u>R</u>

CITY OF BELLEVILLE
Joseph Reid, General Manager
Transportation and Operations Services
Report No. GMTOS-2021-02
February 8, 2021

To: Mayor and Members of Council

Subject: Outdoor Recreational Facility Update

Recommendation:

“THAT pursuant to the General Manager of Transportation and Operations Services Report No. GMTOS-2021-02, Council receive the Outdoor Recreational Facility Update and direct management to proceed with the planning stage of the Project at the former QER Fairgrounds.”

Strategic Plan Alignment:

The City of Belleville’s Strategic Plan identifies nine strategic themes. This report aligns with the “Tourism and Waterfront Revitalization” strategic theme to “Promote and support the development of attractions, events, facilities and services that will draw visitors to the community” and the “Culture and Recreation” theme “to plan and develop a parks system with facilities and services that promote health and wellness” and “promote beautification of the community through excellence in urban design”.

Background:

The City of Belleville received a project proposal from the Rotary Club of Quinte to construct an Outdoor Recreational Facility within the City. Rotary Outdoor Recreational Fundraising Co-Chairs, Ryan Lapalm and Cassandra Bonn, along with President Tim McKinney have committed to raising funds for this project. The Rotary Club of Quinte has been a significant partner in other City projects, such as the Fitness Park and the Musical Park.

In preliminary discussion with Sierra Planning, who is completing the Parks and Recreation Master Plan, it has been identified that an Outdoor Recreational facility additional outdoor recreation facilities represent an immediate (short-term) need for our community. These requirements comprise a range of outdoor amenities from courts (Basketball, tennis, pickleball and multi-court) to innovative forms of public enjoyment of parks and open spaces including outdoor fitness, playgrounds, and trail connections. There is an opportunity to develop a defined outdoor recreation hub serving the City as a whole, while also providing in-park amenities elsewhere that serve neighbourhood and district needs.

An outdoor recreation facility hub would include a number of facilities co-located for maximum community benefit, and may include mechanical ice, multi-court facilities to meet the need for basketball as well as other court sports, and seasonal use opportunities such as ball hockey, lacrosse, cricket, and a host of potential other activities.

Seirra Planning also noted that accordingly the location for a hub should be determined based on the potential synergies in terms of capital investment, operational costs, and potential community and economic benefit.

Financial/Analysis:

During the 2020 Capital Budget deliberations, City Council approved Item 20-1.057 for Outdoor Recreation Facility Design in the amount of \$50,000. A location for such facility has not been earmarked to allow staff to commence site design to date.

City Staff along with Rotary Co-Chairs have reviewed various locations throughout the City for this facility. Sites were looked at in the east end, west end and north of the 401; including sites such as Bird Park, Alemite Park, and QER Fairgrounds. It should be noted that this will be the first location of such a facility, other sites in the City could be developed in the future.

While all sites have significant merit, the location at Bridge and Sidney Street stood out, specifically where the existing grandstand is situated. As an agreement between the City and the Belleville Agriculture Society has been reached for a new site, this location will soon be vacated. This site is on a public transit route, which is optimal, but the most significant advantage is that it has been earmarked for the new YMCA build. The synergies that could be achieved are incredible with programming and joint use. As well, the close proximity to the Curling Club is of benefit as well with the potential of joint refrigeration in the future. This will allow the Sidney Street frontage open for City Council to choose at a later date.

By selecting the location for the Outdoor Recreation Facility, staff can begin the site design and planning the foot print. This direction will help staff determine the full scope of the project; to be present at 2022 capital budget deliberations.

Further, by choosing a location, the Rotary Club will get a jump start on this capital build campaign to assist with funding the facility, with City support.

Conclusion:

The QER Fairgrounds have been identified as the most optimal location for the newly proposed Outdoor Recreational Facility, in partnership with the Rotary Club of Quinte. Management is looking for Council approval and direction to

proceed with the planning stage of the Outdoor Recreational Facility at the former QER Fairgrounds.

Respectfully Submitted,



Joseph Reid

General Manager, Transportation and Operations Services



APPROVAL BLOCK	
CAO	<u>RS</u>
DF	<u>MA</u>
GMTOS	<u>CF</u>
PS	<u>CF</u>

CITY OF BELLEVILLE
Paul Buck, Manager of Transit Services
Transportation and Operations Services
Report No. MT-2021-01
February 8, 2021

To: Mayor and Members of Council

Subject: Supply and Delivery of five (5) Canadian Odyssey Fareboxes

Recommendation:

“THAT in accordance with Section 30.3 Sole and Single Sourcing – Approval and Reporting of the City’s Purchasing By-law Number 2020-09, the quotation from Garival S.E.C. be accepted for the purchase of five (5) Canadian Odyssey Fareboxes for the new Transit Buses, in the amount of \$113,350.00 plus \$14,735.50 HST, for a total of \$128,085.50, and that the Mayor and City Clerk be authorized to sign the Acceptance Agreement on behalf of the Corporation of the City of Belleville and the City Clerk be authorized to affix the Corporate Seal.”

Strategic Plan Alignment:

The City of Belleville’s Strategic Plan identifies nine strategic themes. This report aligns with the “Transportation and Mobility” strategic theme to “Develop a viable, affordable and accessible public transit system that addresses the needs of our citizens”.

Background

City Council received Investing in Canada Infrastructure Program (ICIP) funds to purchase new 5 new transit buses, which will be delivered in March 2021. 2019 Capital Budget Issue 1.110 was approved as ICIP-BEL-02 and included the purchase of technology and fareboxes, with an estimated cost of \$115,000.00.

The fareboxes permit non-contact fare media payment by use of coins, monthly passes and electronic fare payment (B-TAP), and permit non-contact issuing of transfers to customers. These fareboxes also collect and provide significant ridership data for Transit Operations.

Financial/Analysis

In keeping with the City's Purchasing Policy Section 30.2, this project qualifies as "single sourcing" whereby there are valid and sufficient reasons for selecting one supplier, as follows:

- *For additional orders/deliveries by the original supplier of goods or services that were not included in the initial procurement if a change of supplier for such additional goods or services: (a) cannot be made for economic or technical reasons such as requirements of interchangeability or interoperability with existing equipment, software, services or installations procured under the initial procurement; and (b) would cause significant inconvenience or substantial duplication of costs for the procuring entity.*

In December 2017, City council approved an investment of \$485,000 for Transit Bus Farebox upgrades, partially funded by PTIF, and 17 Genfare fareboxes were purchased from Garival through Sole Source procurement.

The Canadian Odyssey fareboxes from Garival are completely compatible with the departments' systems. The fareboxes are equipped with the newest technology available; ensuring fast and accurate fare transactions and accurate ridership data to better monitor and enhance services to residents.

The quote received from Garival S.E.C. is as follows:

Vendor	Quote Price	HST at 13%	Total Amount	Net Cost to City*	Budget Amount
Garival S.E.C.	\$113,350.00	\$14,735.50	\$128,085.50	\$115,344.96	\$115,000.00

* Net Cost to City including HST rebates

Sufficient funding remains within 2019 Capital Budget Issue 1.110 to accommodate the net cost to the City of \$115,344.96 for the purchase of these fareboxes.

In recommending the single source purchase of the five (5) Canadian Odyssey Fareboxes, Management is satisfied that the value for service and long term cost savings will be achieved.

Conclusion

In adherence to Section 30.3 Sole and Single Sourcing – Approval and Reporting of the City’s Purchasing By-law Number 2020-09, it is recommended that the Canadian Odyssey Fareboxes be purchased from Garival S.E.C. The Fareboxes are expected 90 to 120 days from issuance of the purchase order.

Respectfully Submitted,



Paul Buck
Manager, Transit Services

Attachments

- Schedule 1 – Single Source Checklist
- Schedule 2 – Garival quote

CITY OF BELLEVILLE
SINGLE/SOLE SOURCING CHECKLIST

This checklist is a guideline to assist in determining if a purchase of goods and/or services qualifies as a single or sole source purchase. If further clarification is required please contact Purchasing Services.

Single and Sole Source purchases must be made in accordance with section 30.1 and 30.2 of the Procurement Policy.

Purchasing Services shall assist the Department in making the determination that a purchase meets the criteria of a sole or single source purchase by the following process:

1. Single Sourcing: (check appropriate item below)

Single source means that there is more than one source of supply in the open market, but only one source is recommended due to predetermined and approved specifications.

The procurement may be conducted using a single Source process if the goods and/or services are available from more than one source but there are valid and sufficient reasons for selecting one supplier in particular, as follows:

- An attempt to acquire the required goods and/or services by soliciting competitive bids has been made in good faith, but has failed to identify any, willing and compliant, competitive suppliers;
- For additional orders/deliveries by the original supplier of goods or services that were not included in the initial procurement if a change of supplier for such additional goods or services: (a) cannot be made for economic or technical reasons such as requirements of interchangeability or interoperability with existing equipment, software, services or installations procured under the initial procurement; and (b) would cause significant inconvenience or substantial duplication of costs for the procuring entity;
- for purchases made under exceptionally advantageous conditions that only arise in the very short term in the case of unusual disposals such as those arising from liquidation, auction, receivership or bankruptcy, but not for routine purchases from regular suppliers;
- It is advantageous to the City to acquire the goods and/or services from a supplier pursuant to the procurement process conducted by another public body;
- It is advantageous to the City to acquire the goods and/or services directly from another public body or public service body;
- Another organization is funding or substantially funding the acquisition and has determined the supplier, and the terms and conditions of the commitment into which the City will enter are acceptable to the City;
- Where due to abnormal market conditions, the goods and/or services required are in short supply.
- Section 21. from the Purchasing by-law where the goods and/or services are considered an extension of a previous Purchase using a competitive Bid process and the previous supplier would be considered appropriate as a single source procurement due to the particular nature of the extension.

SUMMARY OF RATIONALE:

Purchasing additional fareboxes for five (5) new buses.

These are identical make and model of all other fareboxes.

Name: Paul Buck

Signature: 

Date: February 1, 2021

2. Sole Sourcing: (check appropriate item below)

The procurement may be conducted using a Sole Source process if the goods and/or services are available from only one supplier by reason of:

- Statutory or market based monopoly;
- Competition is precluded due to the application of any Act or legislation or because of the existence of patent rights, copyrights, license, technical secrets or controls of raw material; or
- The complete item, service, or system is unique to one supplier and no alternative or substitute exists.

SUMMARY OF RATIONALE

Charge Account Number: _____

Name: _____

Signature: _____

Date: _____

3. Approval and Reporting (Single and Sole Sourcing)

Purchases which qualify to be considered as a Single Source or Sole Source require the following approvals:

- For purchases **under \$20,000** a completed Single/Sole Sourcing Checklist, including any other applicable official documents (if required) indicating the compelling rationale that warrants a non-competitive selection and all documents will be attached to the **purchase order which shall be issued by the Department.**
- For purchases **greater than \$20,000 but less than \$50,000** a completed Single/Sole Sourcing Checklist, including any other applicable official documents (if required) indicating the compelling rationale that warrants a non-competitive selection and all documents will be attached to the **purchase order which shall be issued by Purchasing Services.**
- **For purchases over \$50,000** a written report indicating the compelling rationale (shall include a copy of a completed Single/Sole Sourcing Checklist) that warrants a non-competitive selection will be submitted by the **Department to City Council for approval** prior to selection. A copy the approved official documents (if applicable) shall be attached directly to the purchase order issued by Purchasing Services.



Estimate			
Doc.#	Customer	Date	Page
35524	BELL	2021-01-28	1

Sold to
Belleville Transit FINANCE DEPARTMENT 400 Coleman Street Belleville ON K8P 3J4

Ship to
Belleville Transit C/O: PAUL BUCK 400 Coleman Street Belleville ON K8P 3J4

Purchase Order no.	Terms	Ship Via	F.O.B.	Ref. #
	Net 30		Laval	35524
Quantity	Item	Description	Unit Price	Total
Ordered Shipped B.O.				
5	C24003-BELL	CANADIAN ODYSSEY FAREBOX 41" BELLEVILLE SPECS	22 670,00 \$	113 350,00 \$

Notes:

- Delivery lead time: 90-120 days upon receipt of a purchase order
- Installation not included
- Freight not included
- Warranty One (1) Year

TVH / HST 13% 14 735,50 \$

GARIVAL S.E.C.	TPS / GST # 71710 7874 RT0001 TVQ / PST # 1227727744 TQ0001	Subtotal	113 350,00 \$
Note : Les prix sont valides pour une période de 30 jours / Prices are valid for a period of 30 days 31689		Freight	0,00 \$
		Tax	14 735,50 \$
		Total Amount	128 085,50 \$
		Amount Received	0,00 \$
		Balance	128 085,50 \$